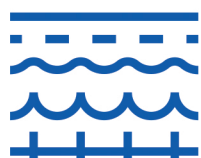
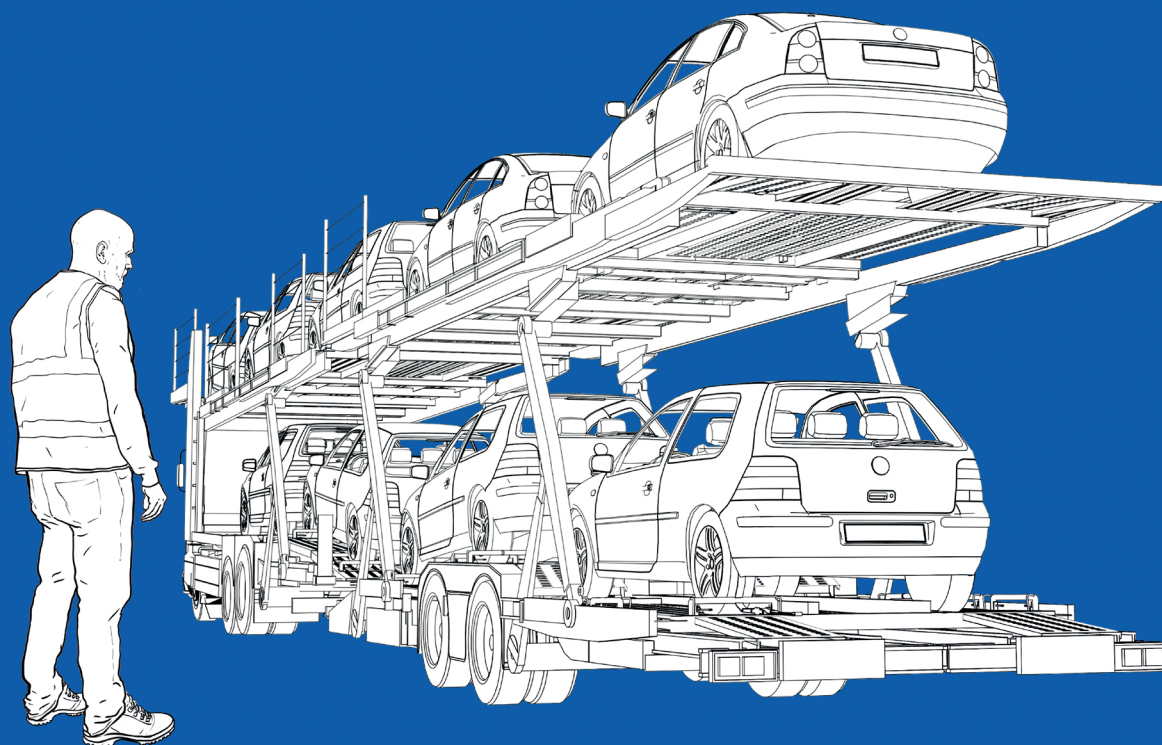


ECG Guidelines

Safe loading process

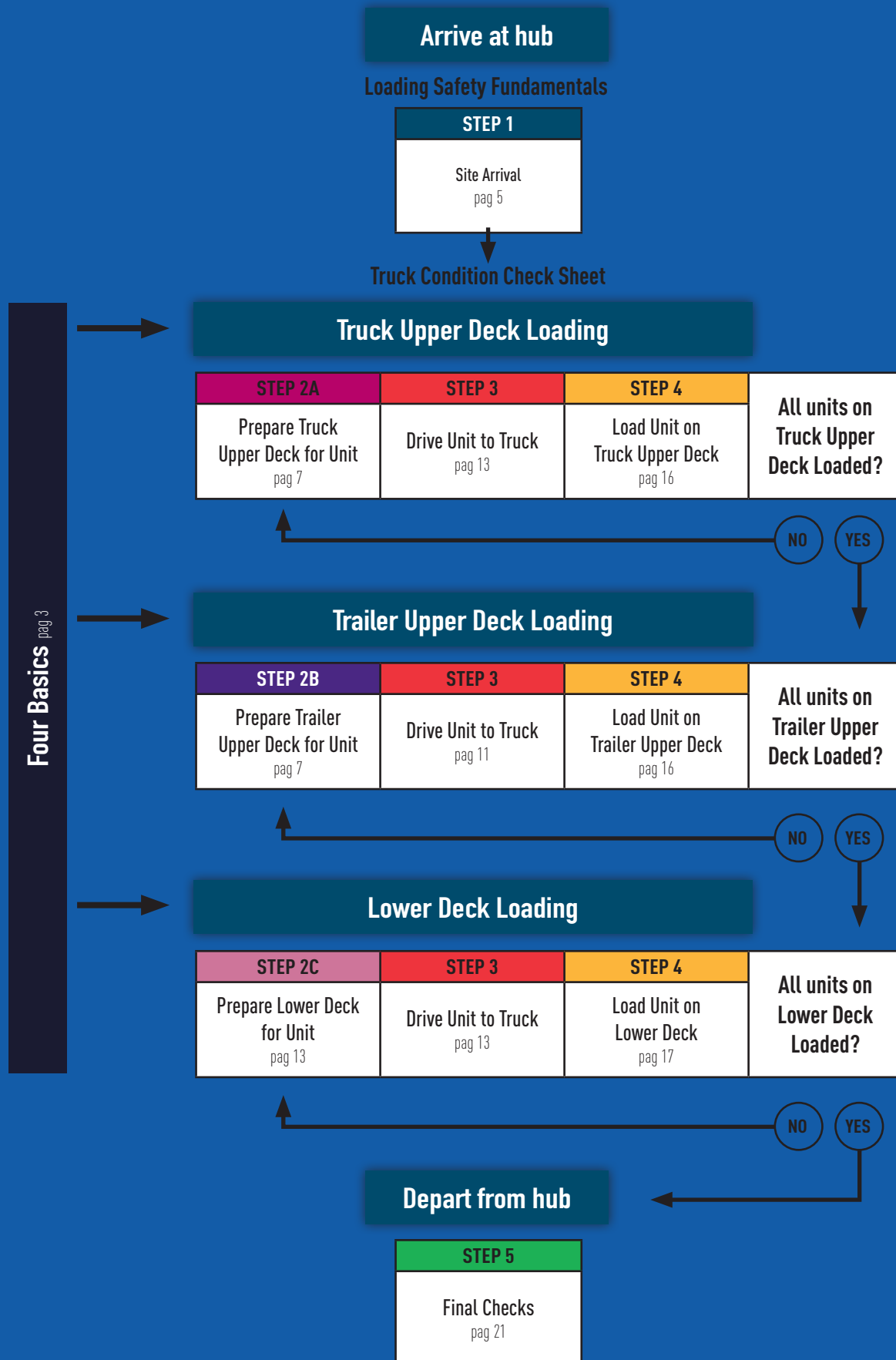
VERSION 1, MAY 2020



ECG

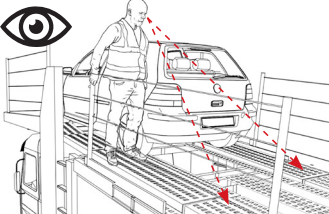
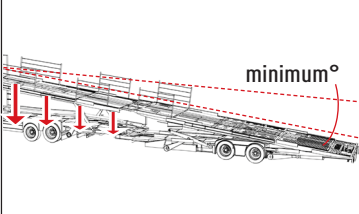
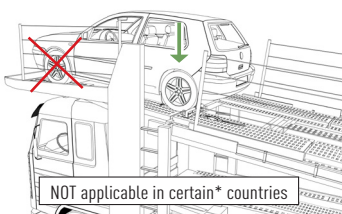
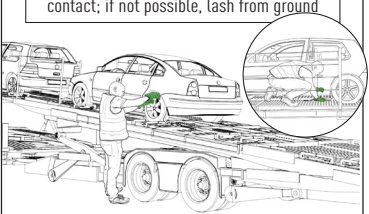
The Association
of European
Vehicle Logistics

Overview of Loading Process

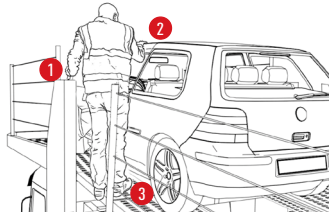
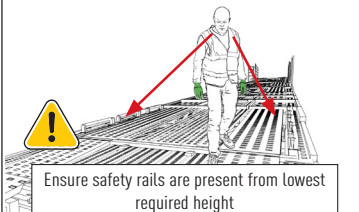
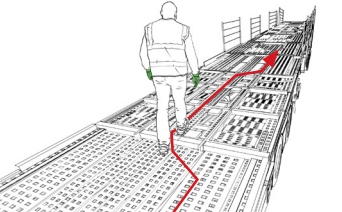
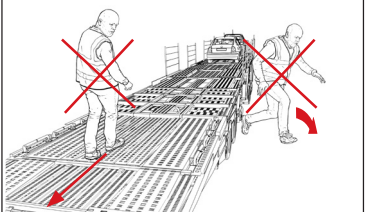


Loading Safety Fundamentals

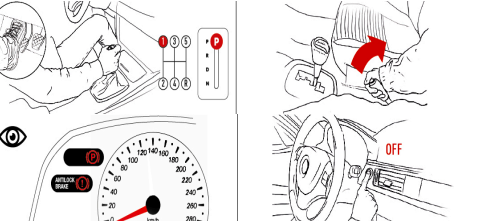
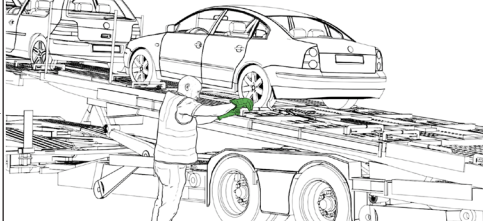
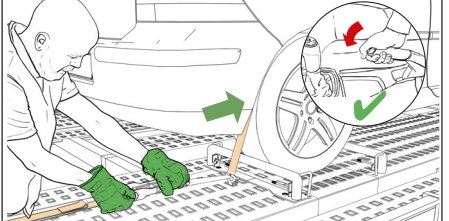
1. Lash safely to prevent falls-from-height

			<p>Cars on truck upper deck ONLY: ensure body is entirely between safety rails with 3 point contact; if not possible, lash from ground</p> 
<p>A. Visually confirm safety rails are present & damage-free</p>	<p>B. Lower decks as much as possible when loading top decks</p>	<p>C. Never lash axle closest to front-end of truck</p>	<p>D. Cars on trailer upper deck & entire lower deck: lash from ground.</p>

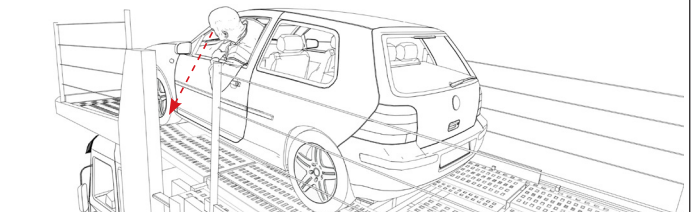
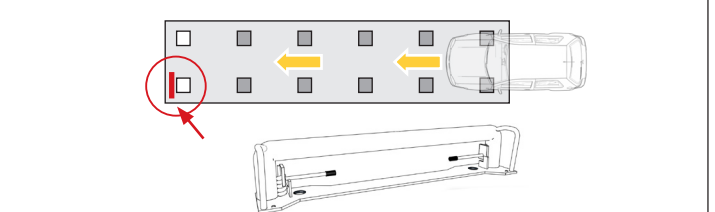
2. Walk carefully to avoid Slips & Trips

			
<p>A. Use 3 points of contact when moving on upper deck</p>	<p>B. Check + prepare decks to avoid walking near holes, chocks or lashes</p>	<p>C. Walk on designated safe areas on the truck as specified by truck manufacturer</p>	<p>D. NEVER run, jump or walk backwards.</p>

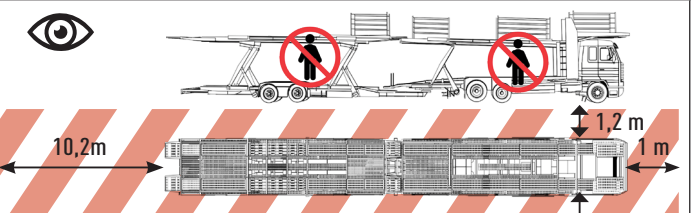
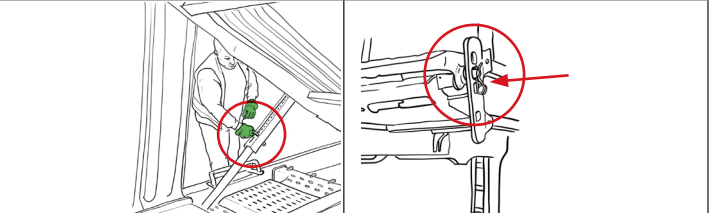
3. Confirm parking + chock on exit + lash before releasing car to prevent roll-offs

		
<p>A. Always follow 4-step parking process & CONFIRM car is securely parked</p>	<p>B. Chock car in direction of slope, as soon as possible after exiting</p>	<p>C. Lash fixed axle BEFORE releasing brake/gear in unit for deck manipulation</p>

4. Use chocks/drop-holes + visual judgement to reduce drive-off risk

	
<p>A. Drive against a chock or open a drop-hole to physically prevent drive-off (if allowed by OEM/yard operator)</p>	<p>B. Visually confirm alignment to truck centre-line & stopping position</p>

5. Look & Lock to avoid entrapment in truck

	
<p>A. Ensure no one is on/around truck before manipulating decks, ramps or suspension</p>	<p>B. Ensure all safety pins & levers are locked</p>

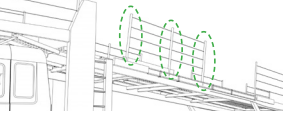
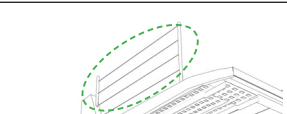

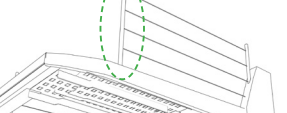

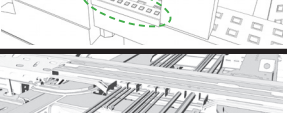

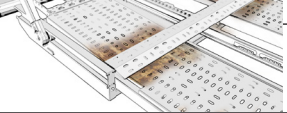
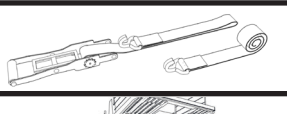

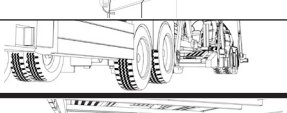

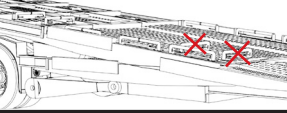





*In certain countries (e.g. Germany) there is a legal requirement to lash diagonal axles of car in the first position. For trucks loading/unloading in such countries, recommendation 1C is not applicable. In such cases, drivers can lash the foremost axle as long as they stand entirely between safety rails, facing the front of the truck.

Pre-Loading Check Sheet

Truck plate:

Site:

Check Date:

Item	Area	OK Criteria	Judgement Method	Example of OK condition	Penalty	OK?
SAFETY RAILS	Fence/fence holders	All installed	Visual check for fence-holders "If there is a holder on deck, there must be a pole in it"		Do not allow unit to be loaded in a position where safety rails are NOT OK workshop maintenance before next loading is recommended NOTE: DO NOT ALLOW TO FIX OR STRAIGHTEN BENT POLES	
	Height	Min. 900mm	Visual check Height measure stick if borderline			
	Wires	Protected	Visual/touch check			
		All installed (as per truck manufacturer spec)	Visual check for holes on pole "If there is a hole on a pole, a wire must be going through it"			
		Sagging	Visual check Any wire sagging so low that it is halfway to the next wire is NOT OK			
	Wire Connections	Covered (as per truck manufacturer spec.)	Visual check Any connections not covered are NOT OK, unless supplied so by manufacturer .			
	Poles	No visible corrosion	Visual check Any corrosion on poles (esp. near holes or base) is NOT OK			
		Unbent	Visual check Even a slight bend in poles is NOT OK			
ANTI-SLIP	On ladders	Exists (either rubber strip or perforations)	Visual/touch check		Workshop maintenance before next loading is recommended	
	On walkways					
OIL LEAKAGE	Hydraulics	Not dripping/leaking	Wipe part with clean cloth, and place blank paper below it. If oil drips on it more than once every 30 seconds, it is NOT OK		Do not allow unit to be loaded in a position where there is oil leakage Workshop maintenance before next loading is recommended	
	Moving Parts					
RUST	Deck: Driving/walking areas	Not flaking	If it is possible to break off a piece of metal from any of these areas by hand, it is NOT OK Surface/cosmetic rust is OK, and can be expected on all trucks after some time		Do not allow unit to be loaded in a position where there is a rusty surface, or rusty part Workshop maintenance before next loading is recommended	
	Ladders					
	Moving Parts					
	Load bearing pillars/pins					
LASHES	All lashes	No visible tears, cuts & ISO/EN standard label visible	Visual check. If label not visible, lash is NOT OK. If tear/scratch is more than 5% width/depth, lash is NOT OK		Ask driver to replace lashes	
DRIVING CONDITION	Windshield	No damage that restricts visibility; no other visual obstruction	Visual check. Minor damage (size/position within legal limits) is OK Field of vision obstructions are NOT OK		Workshop maintenance before next loading is recommended	
	All Mirrors	No damage that restricts visibility	Visual check. Any damage (e.g. cracks) which limits use of mirror is NOT OK.			
	Tyres/ Wheels	No damage that restricts driveability	Visual/touch check. Treads must be visible to naked eye. Any deep damage/rubber peeling off tyre is NOT OK.			
OTHER	Truck pillars	Protected w/ foam or rubber on the inside	Visual/touch check		Do not allow unit to be loaded in a position where there are any of these problems (or in any position which requires unit to be driven over sharp surfaces) + warn trucking company to fix problem	
	Deck: Driving/walking areas	No sharp surfaces/edges/torn metal	Visual check			
	All over truck	Loose or hanging cables/hoses/lashes/chocks	Visual check			
		Any modifications/additions/removals that are not mfr.- approved	Visual check			
DRIVER	Safety Shoes	Driver wearing EN20345 S1P (or S3) + SRB anti-slip std. safety shoes	Visual check		"Request driver to rectify. If not possible for driver to rectify, do not allow loading of truck, and inform dispatch office"	
	Hi-viz	Driver wearing EN20471:2013 Class 2 hi-viz that covers entire torso				
	Gloves	Driver uses EN388:2016 gloves for lashing/chocking/re-arranging deck elements				
	Jewellery/ Metal/ Sharp Objects	Driver has no exposed jewellery, metal or sharp objects on clothing/body			"Request driver to rectify. If not possible for driver to rectify, warn driver, and inform dispatch office"	
	Clothes/ hands	Clothes & hands are reasonably clean				

Four Basics

1. ENTERING UNIT	A. Open door with 3 PC (1 hand on door edge, 1 hand on roof, feet on deck/ ground)	B. Sit on driver-seat with both legs outside & knees pointing towards door	C. Swivel body & bring legs into the unit
	To enter the unit safely & without damaging door	To prevent ergonomic injuries when entering vehicle	To prevent ergonomic injuries when entering vehicle

2. RELEASE UNIT ON THE GROUND	A. Secure unit	B. Turn on Engine	C. Engage gear to drive
	To prevent drive off	To move unit	(C) To engage driving axle
	D. Release parking brake		F. Confirm there are no parking brake lights on dashboard
	To free the rear axle		To prevent damage to vehicle whilst driving

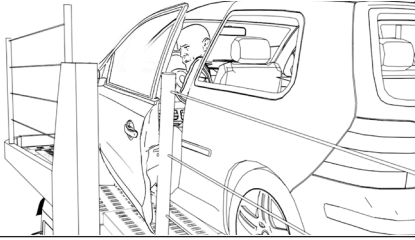
3. 4-STEP PARKING	A. Put Gear in 1st/P For manual keep foot on clutch until engine is switched off	B. Apply Parking brake
	To lock the drive axle and prevent vehicle roll off	To prevent unit rolling off
	C. Check parking-brake light on odometer is ON	D. Turn off engine
	To ensure parking-brake is applied	To preserve fuel during loading & transport

Four Basics

Continued

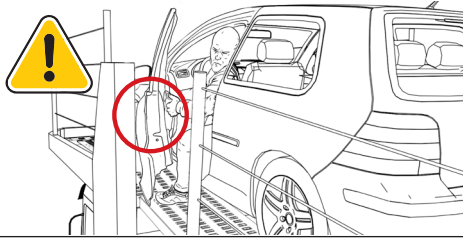
4. EXITING UNIT ON DECK

A. Open door slowly



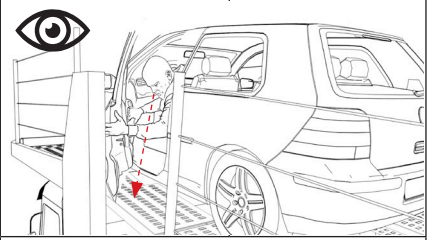
To prevent door bumping against truck structure

B. Stop opening door before touching the safety pole



To prevent damage to door when exiting

C. Look through open door at deck area around unit



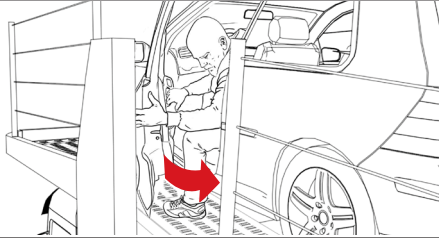
To ensure step-out on to safe area

D. Place one foot on deck (Left foot for LHD, right foot for RHD units)



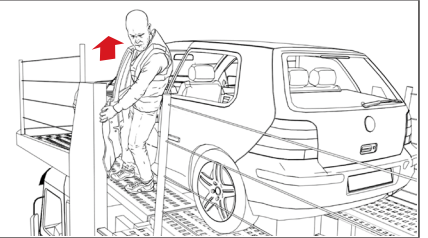
To begin exiting unit safely

E. Swivel body and bring both feet on deck



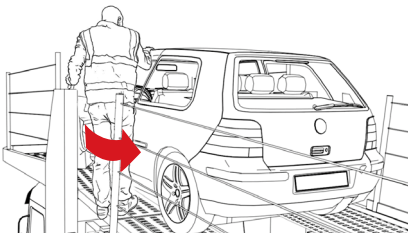
To begin exiting unit safely

F. Stand up maintaining 3 points of contact



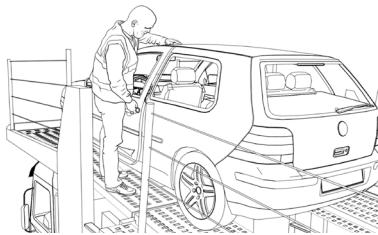
To prevent slips trips or falls

G. Rotate body 180° while between open door & unit



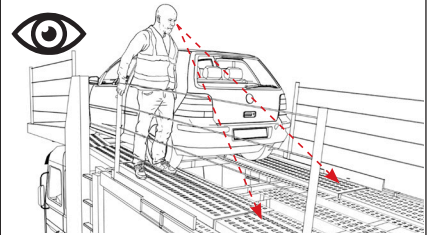
To close the door safely

H. Close door carefully using 3 points of contact





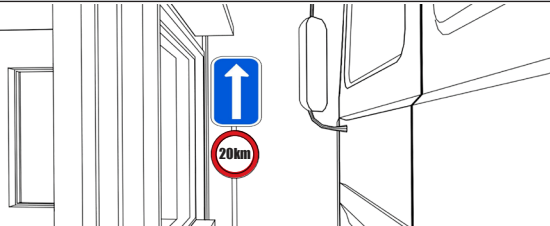
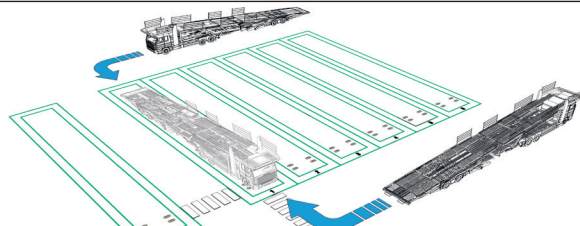
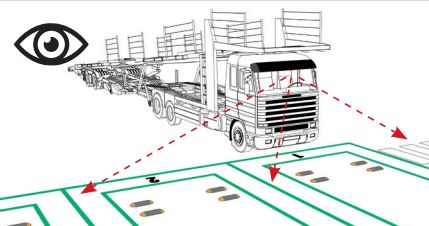

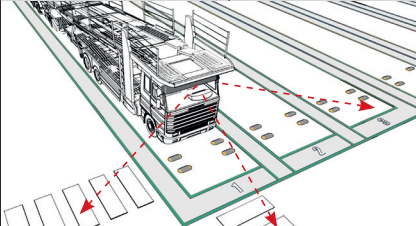
To prevent slips, trips or falls

I. Maintain 3 points of contact and check surroundings



To prevent slips, trips or falls

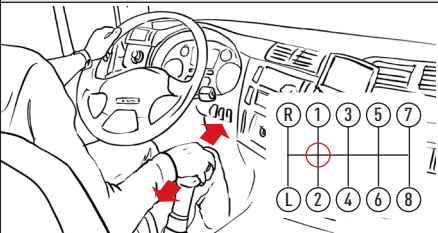
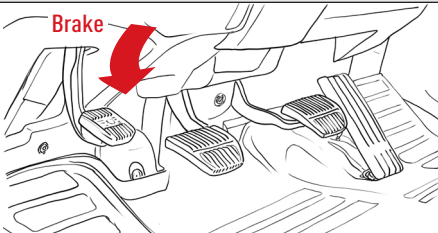
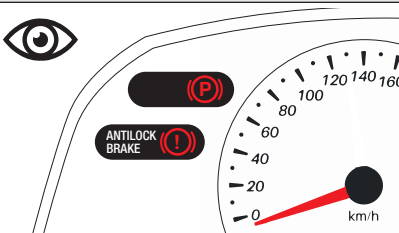
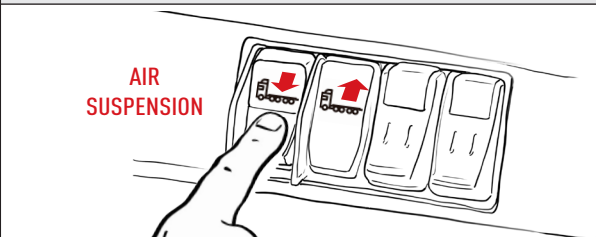
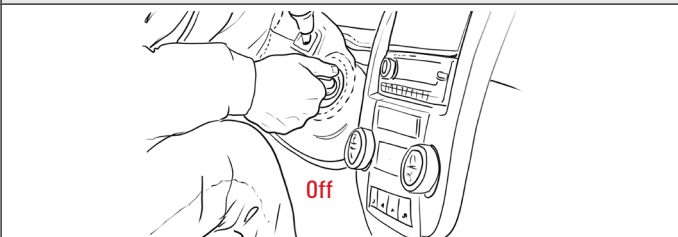
Step 1 – Arrival on Site

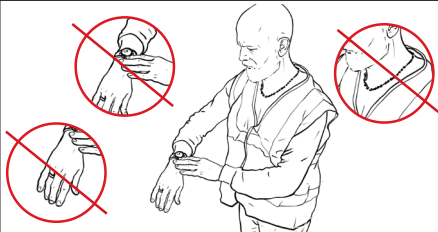

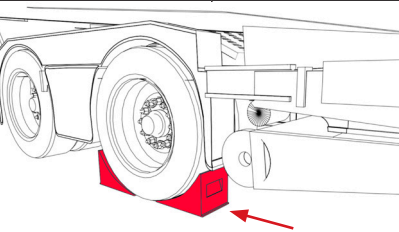
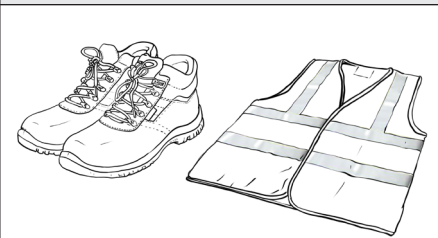
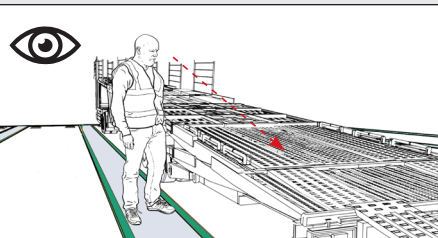

1.1 ARRIVE AT SECURITY CHECK POINT	A. Stop at check point	B. Follow instructions	
	 <p>To receive site instructions</p>	 <p>To enter site safely</p>	
1.2 DRIVE TO LOADING ZONE	A. Respect traffic rules when driving into compound	B. Identify correct parking method (reverse/drive in)	
	 <p>To prevent accidents during driving</p>	 <p>To park the truck</p>	
1.3 MANOEUVRE INTO BAY	A. Check visually no pedestrians are in the manoeuvring area	B. Turn on hazard lights	C. Check surroundings while manoeuvring
	 <p>To minimize risk of truck-man collisions</p>	 <p>To warn other trucks/pedestrians of manoeuvre</p>	 <p>To minimize risk of truck-truck or truck-man collisions</p>

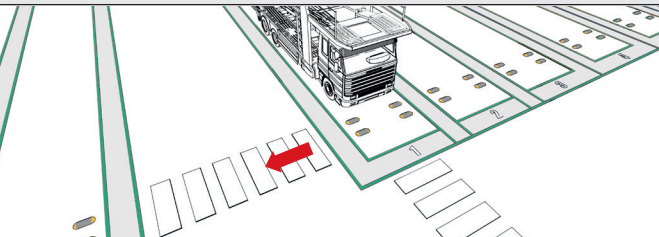
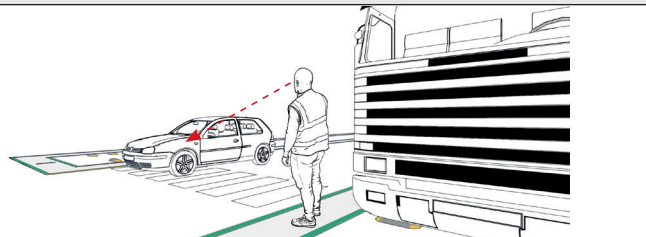
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Step 1 – Arrival on Site

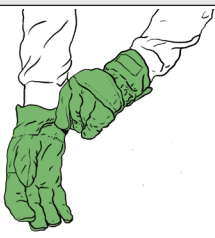
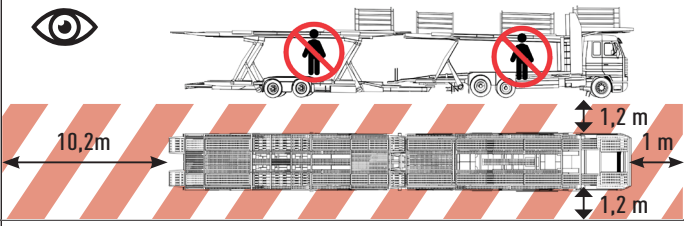
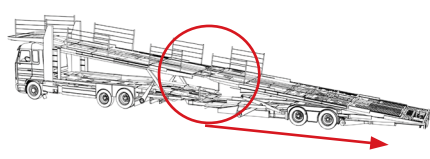
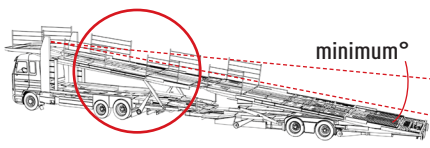
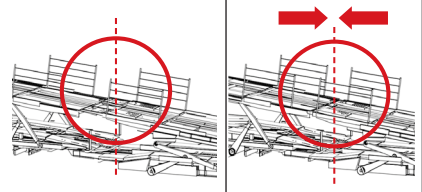
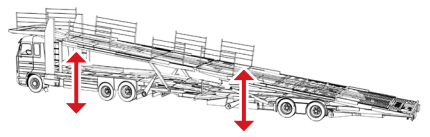
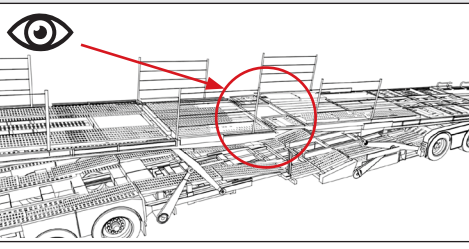
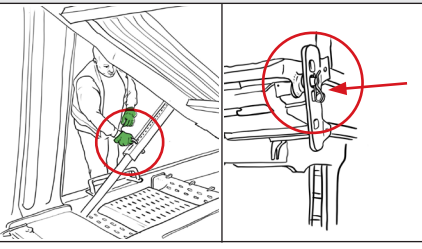
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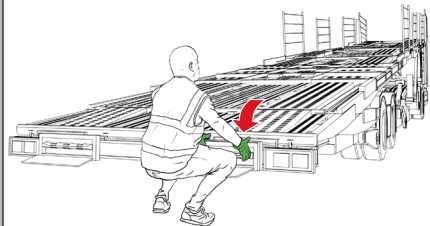
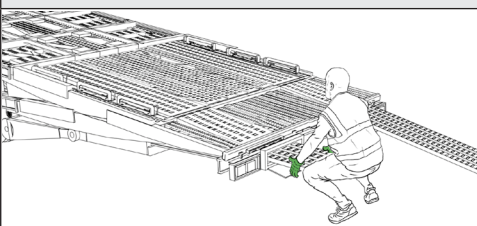
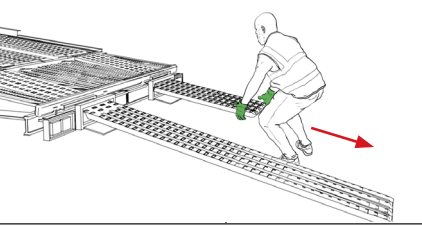
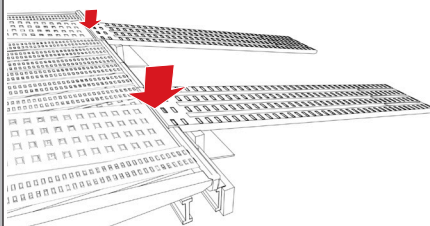
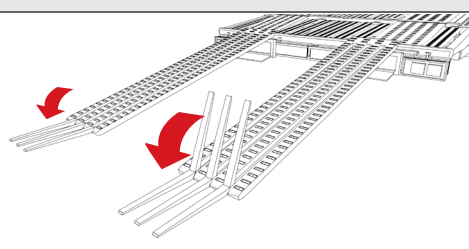
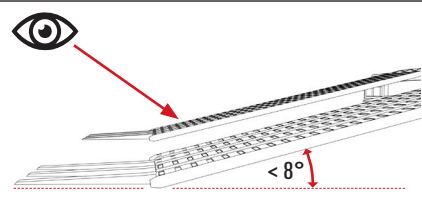
1.4 SECURELY PARK THE TRUCK	A. Put in Neutral	B. Engage parking brake	C. Check parking light on truck odometer
			
	To block an axle and prevent truck roll-off	To block other axle and prevent truck roll-off	To ensure truck is securely parked
	D. Adjust air suspension	E. Turn-off engine (only if the truck has a remote start/stop beside the levers)	
			
	To ensure truck is ready for loading	To minimize emissions during loading	

1.5 CONDUCT ARRIVAL CHECK	A. Remove/cover sharp objects, jewellery or metal	B. Exit cabin using 3 Points of Contact	C. Secure wheel behind truck wheel, if wheel humps are not present in the hub
			
	To prevent damage to units	To prevent slips, trips or falls	To prevent truck roll off
	D. Wear safety shoes & hi-viz jacket	E. Visually inspect safety-rails, hydraulics and decks	F. Check that the truck and driver conditions are in line (see page 2)
			
	To prevent slips, trips, falls; to prevent car-man collisions	To ensure truck is safe to load units on to	To ensure safe loading conditions

1.6 GO TO LOADING OFFICES	A. Use pedestrian path	B. Give priority to vehicles at all crossings
		
	To prevent truck-man or unit-man collisions	To prevent truck-man or unit-man collisions

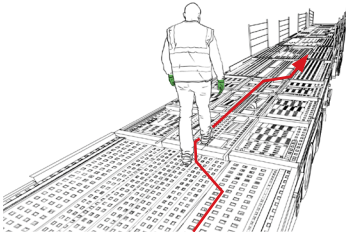
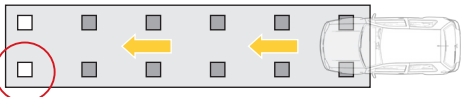
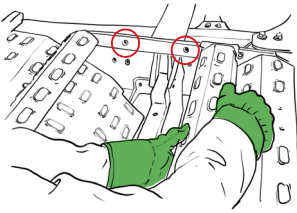
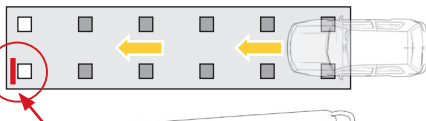
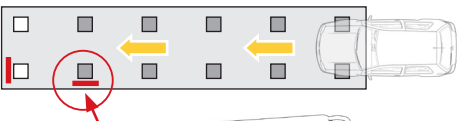
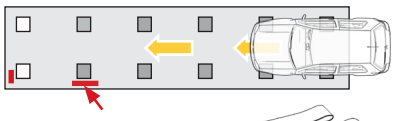
Step 2A – Prepare Truck Upper Deck

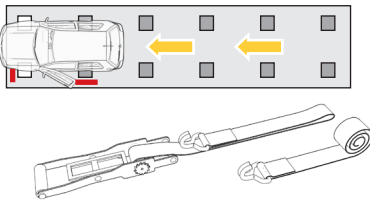
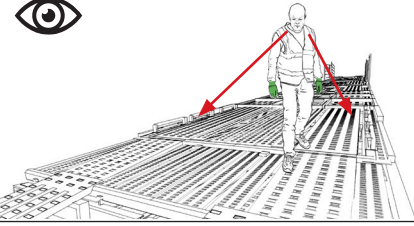
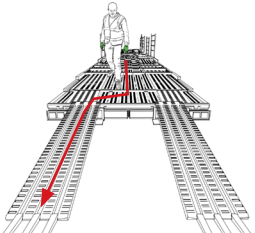

2A.1 ADJUST DECKS	A. Put on gloves		B. Visually confirm that there are no people on or around the truck	
				
	To prevent hand injuries		To prevent injuries to other people in the loading area	
	C. Extend the rear-deck as far out as possible	D. Adjust front truck deck to be as flat as possible	E. Close the gap between truck & trailer decks	
				
	To minimize ramp angle	To prevent roll-off when exiting a loaded unit	To provide a smooth transition between decks	
	F. Adjust truck/trailer suspension using hydraulic levers	G. Visually re-check the gap between truck & trailer decks	H. Ensure safety pins & levers on decks are locked	
				
	To confirm truck/trailer decks are optimally positioned for loading	To ensure smooth transition between decks	To prevent deck from collapsing while driving over	

2A.2 FIX RAMPS TO DECK	A. Open loading ramp boxes	B. Ensure you are in an ergonomic position	C. Pull ramps out
			
	To pull out ramps	To prevent injuries	To attach ramps to deck
	D. Attach ramps to deck	E. Open flaps at the end of the ramp	F. Visually confirm ramp angle is less than 8°
			
	To enable unit loading	To ensure unit wheel smoothly transitions from road to ramp	To prevent damage to underbody during loading

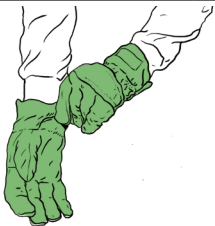
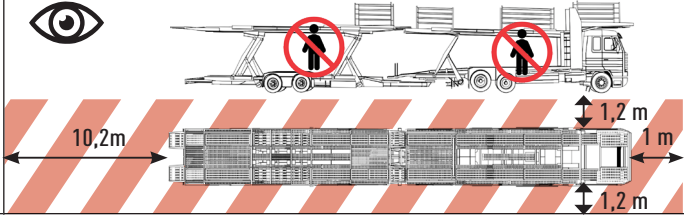
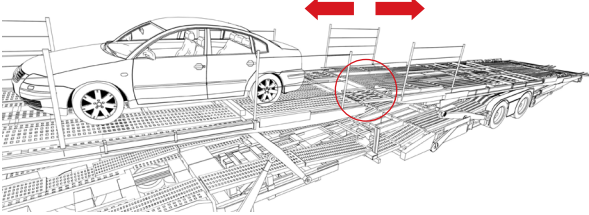
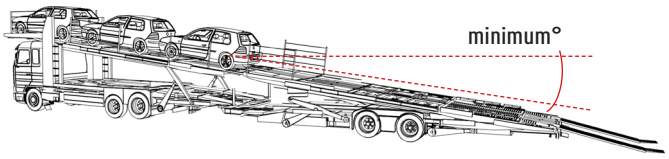
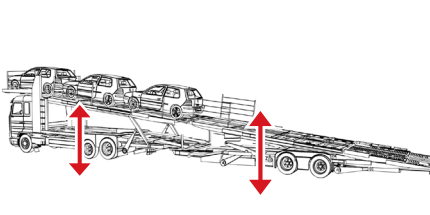
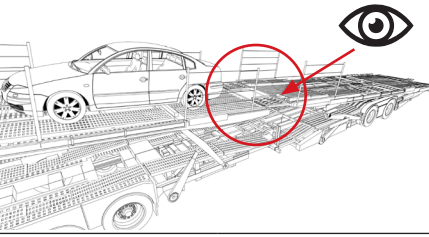
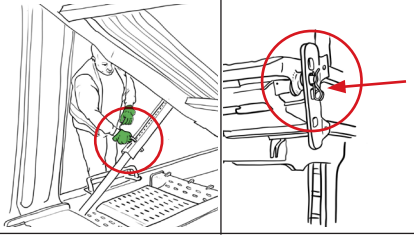
Step 2A – Prepare Truck Upper Deck

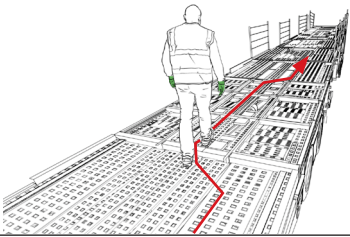
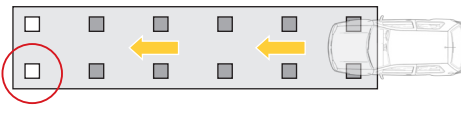
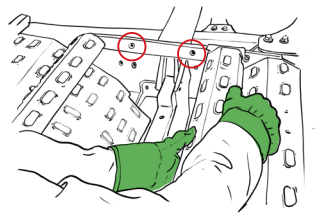
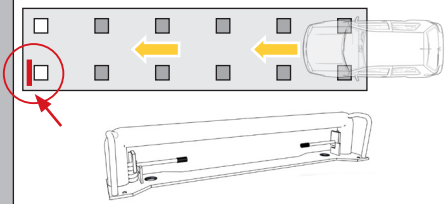
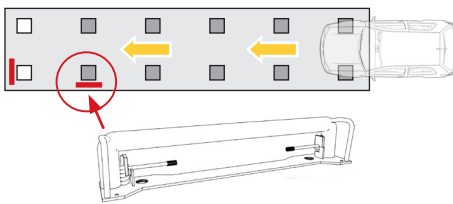
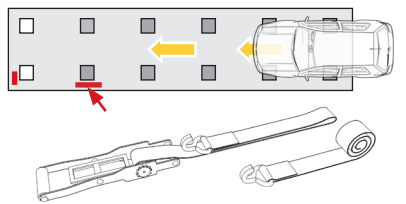
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2A.3 PREPARE DECK FOR UNIT	A. Walk onto deck avoiding open holes, chocks or lashes	B. Close all drop-holes except where unit will be parked	C. Count holes to ensure drop-holes are 1/6th of wheel diameter
			
	To prevent slips, trips or falls	To prevent slips, trips, falls and underbody damage	To prevent roll-off during loading
	D. (Optional) If allowed fix chock to mark parking location of unit (If low clearance, open drop hole instead)	E. Place chock near axle to be fixed	F. Place lash near axle to be fixed
			
	To prevent drive-off	To prevent roll-off by chocking unit immediately after exit	To prevent slips, trips or falls by minimizing walking

2A.4 FETCH UNIT	A. Check & ensure that there is no chock/lash in step-out area	B. Visually ensure chocks & lashes are not in walking or driving path
		
	To prevent slips, trips or falls when exiting the car on deck	To prevent slips, trips, falls and underbody damage
	C. Walk off the deck avoiding open holes, chocks or lashes	D. Remove gloves
		
	To prevent slips, trips or falls	To enter unit without dirtying interior

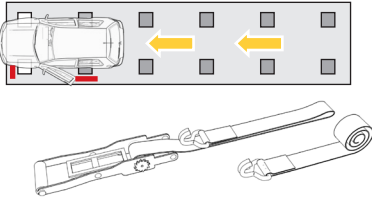
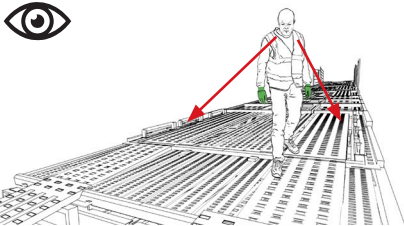
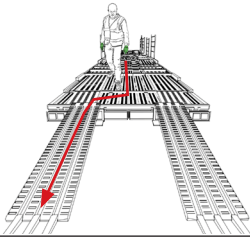
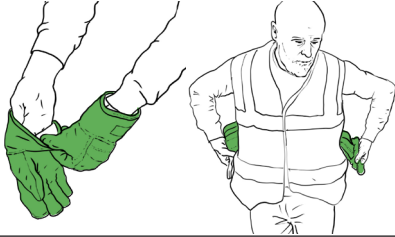
Step 2B – Prepare Trailer Upper Deck

2B.1 ADJUST DECKS	A. Put on gloves		B. Visually confirm that there are no people on or around the truck	
				
	To prevent hand injuries		To prevent injuries to other people in the loading area	
	C. Open gap between truck & trailer decks		D. Adjust trailer deck to be as flat as possible	
				
	To lower height of trailer deck in order to lash from the ground; to estimate unit loading positions on trailer deck		To limit the risk of roll-off when exiting a loaded unit	
	E. Adjust truck/trailer suspension using hydraulic levers	F. Visually re-check the gap between truck & trailer decks	G. Ensure safety pins & levers on decks are locked	
				
	To confirm truck/trailer decks are optimally positioned for loading	To confirm that the gap is adequately open	To prevent deck from collapsing while driving over	


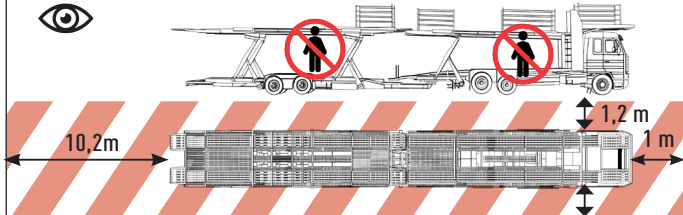
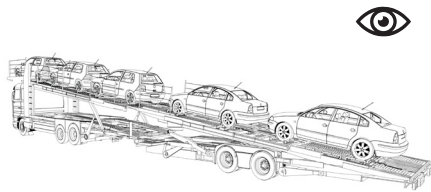
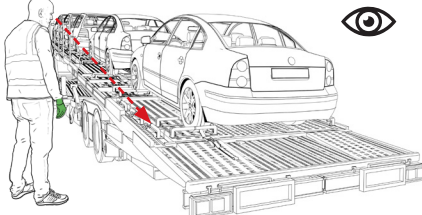
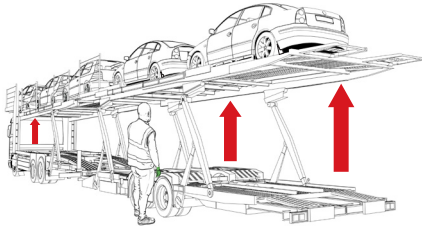
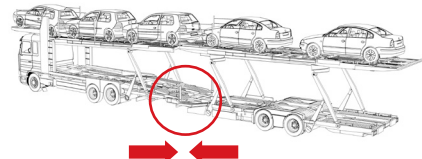
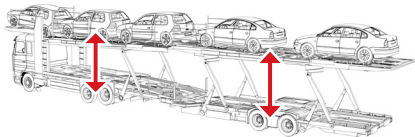
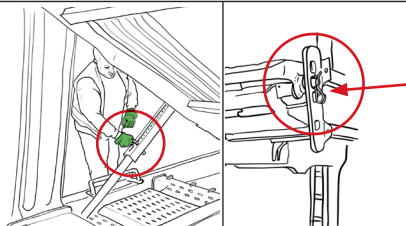
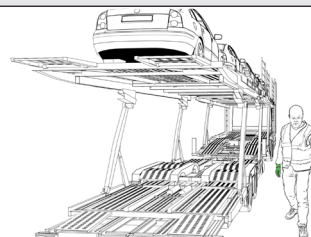
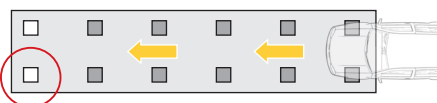
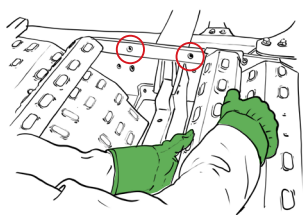
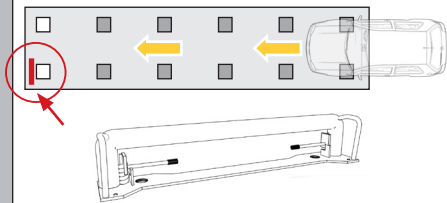
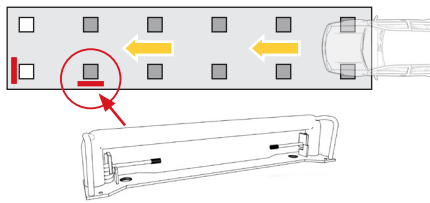
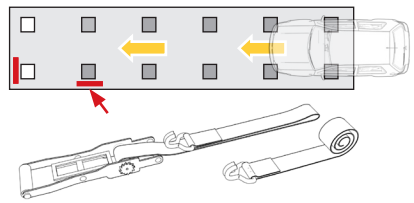
2A.3 PREPARE DECK FOR UNIT	A. Walk onto deck avoiding open holes, chocks or lashes	B. Close all drop-holes except where unit will be parked	C. Count holes to ensure drop-holes are 1/6th of wheel diameter
			
	To prevent slips, trips or falls	To prevent slips, trips, falls and underbody damage	To prevent roll-off during loading
	D. (Optional) If allowed fix chock to mark parking location of unit (If low clearance, open drop hole instead)	E. Place chock near axle to be fixed	F. Place lash near axle to be fixed
			
	To prevent drive-off	To prevent roll-off by chocking unit immediately after exit	To prevent slips, trips or falls by minimizing walking

Step 2B – Prepare Trailer Upper Deck

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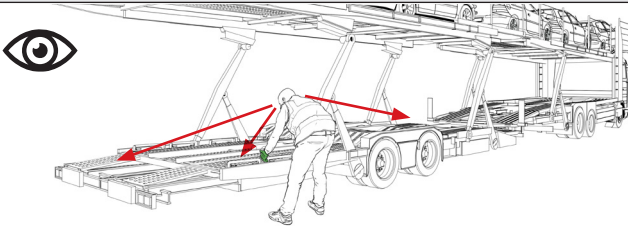

2A.4 FETCH UNIT	A. Check & ensure that there is no chock/lash in step-out area	B. Visually ensure chocks & lashes are not in walking or driving path
		
	To prevent slips, trips or falls when exiting the car on deck	To prevent slips, trips, falls and underbody damage
	C. Walk off the deck avoiding open holes, chocks or lashes	D. Remove gloves
		
	To prevent slips, trips or falls	To enter unit without dirtying interior

Step 2C – Prepare Lower Deck

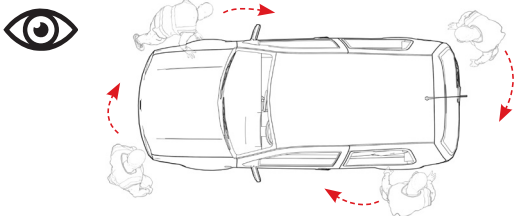
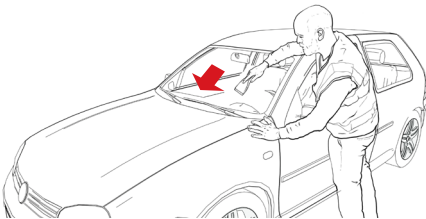
2C.1 ADJUST DECKS	A. Put on gloves		B. Visually confirm that there are no people on or around the truck			
						
	To prevent hand injuries		To prevent injuries to other people in the loading area			
	C. Visually check that all units on upper decks are secured as required		D. Visually check that there are no loose objects on upper deck		E. Raise the upper decks to maximum possible height	
						
	To prevent roll-off		To prevent injuries during manipulation		To prevent head-injuries when loading lower deck	
	F. Close the gap between the truck & trailer lower decks		G. Adjust suspension of truck & trailer		H. Ensure safety pins & levers on decks are locked	
						
	To provide a smooth transition between decks		To provide a smooth transition between decks		To prevent deck from collapsing while driving over	
	2C.2 PREPARE DECK FOR UNIT	A. Safely stand on the ground		B. Close all drop-holes except where unit will be parked		C. Count holes to ensure drop-holes are 1/6th of wheel diameter
						
To prevent slips, trips or falls while preparing lower deck		To prevent slips, trips, falls and underbody damage		To prevent roll-off		
D. Fix chock to mark parking location of unit (If low clearance, open drop hole instead)		E. Place chock near axle to be fixed		F. Place lash near axle to be fixed		
						
To prevent car-car/car-truck collision on deck		To prevent roll-off by chocking unit immediately after exit		To prevent slips, trips or falls by minimizing walking		

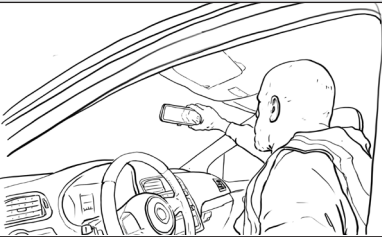

Step 2C – Prepare Lower Deck

Continued

2C.3 FETCH UNIT	A. Visually ensure chocks & lashes are not in walking or driving path	B. Remove gloves
		
	To prevent underbody damage; to prevent slips, trips or falls	To enter unit without dirtying interior

3. Drive Unit to Truck/Trailer

3.1 CHECK THE UNIT EXTERNALLY	A. Visually examine unit exterior	B. Remove ice/snow from windshields & driver-side window at the minimum. Where possible, clear as much ice/snow allowed.
	 <p>To ensure unit is damage free</p>	 <p>To ensure visibility whilst driving</p>

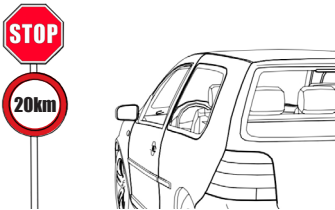
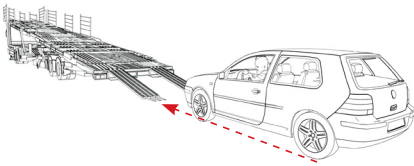

3.2 ENTER UNIT	A. Enter unit See Basic 1	
	B. Adjust rear-view mirror	C. Apply seat-belt (if allowed by OEM/yard operator)
	 <p>To ensure clear rear view</p>	 <p>To minimize injuries in case of collision</p>

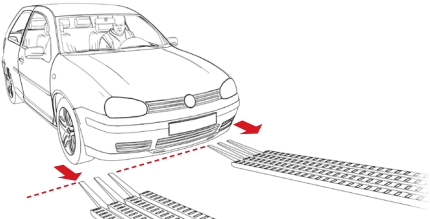

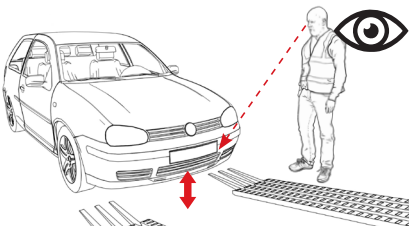
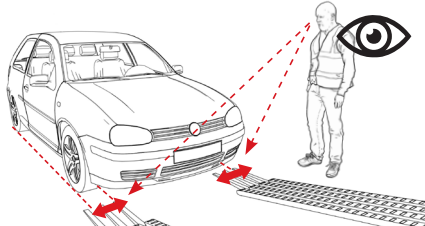
3.3 RELEASE UNIT	Release Unit See Basic 2
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3.4 * IF DRIVING UNIT FOR THE FIRST TIME	A. Enter Unit See Basic 1
	<div>If driving unit for the first time, familiarise yourself with the unit</div> <p>B. Release Unit See Basic 2</p>

3. Drive Unit to Truck/Trailer

Continued


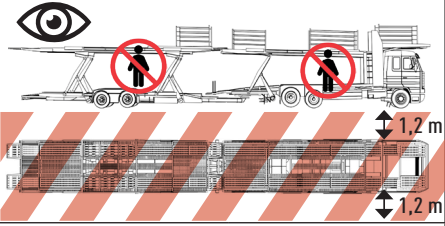
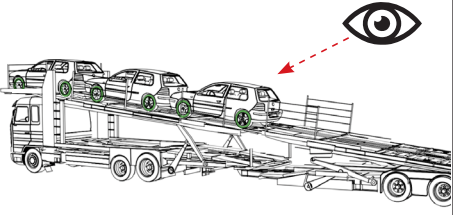
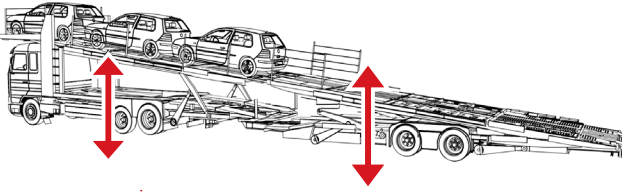
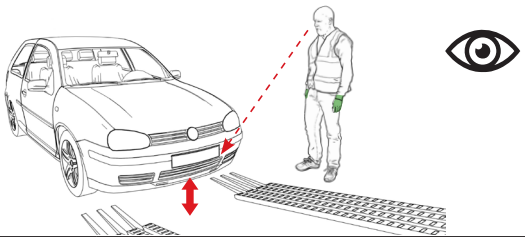
3.5 DRIVE UNIT TO TRUCK	A. Respect all hub driving rules (speed, seatbelt, signs, indicators, etc.)	B. Align unit with loading ramps	C. Unfasten seatbelt and open window*
	 <p>To prevent traffic accidents</p>	 <p>To load safely onto truck</p>	 <p>To prepare to lean out</p>

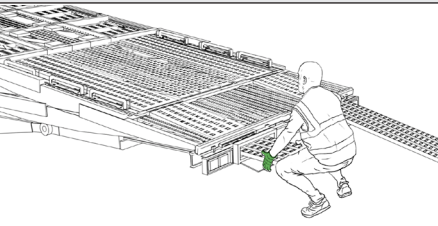
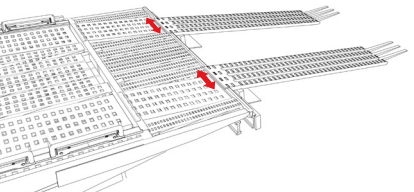
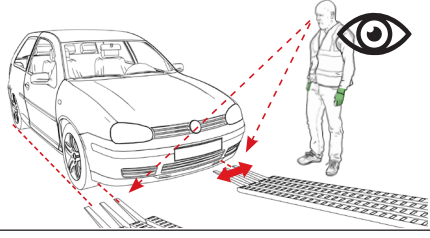
3.6* [IF DRIVING UNIT FOR THE FIRST TIME]	A. Stop as close as possible to loading ramps		B. 4-step parking See Basic 3		
					
	To visually check alignment & clearance				
	C. Exit unit	D. Check underbody clearance	E. Check wheels are aligned with ramps		
					
To visually check alignment & clearance		To prevent damage		To prevent drive-between-ramp accident	

* Please note that some OEMs or LSPs - only for position 1 on upper deck - may require to keep the seat belt fastened.

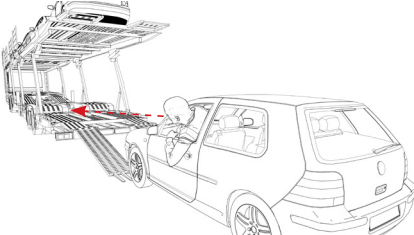
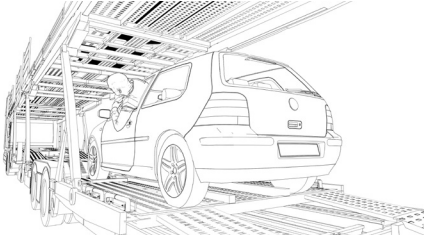
3. Drive Unit to Truck/Trailer

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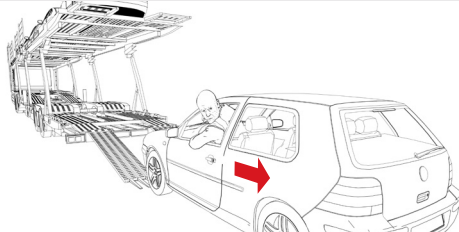



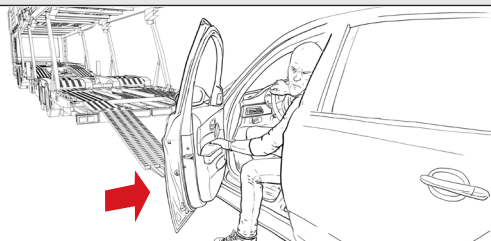
3.7* [IF CLEARANCE NOT OK] ADJUST SUSPENSIONS	A. Put on gloves	B. Visually confirm there is no one on/near truck	C. Confirm any units on deck are properly secured
			
	To prevent hand injuries	To prevent injuries to other people in the loading area	To prevent roll-off accidents
	D. Adjust decks/suspension using hydraulic levers	E. Re-check underbody clearance	
			
	To adjust loading angle of ramps	To prevent underbody damage	

3.8* [IF ALIGNMENT NOT OK]	A. Ensure ergonomic position	B. Adjust ramps	C. Re-check wheels are aligned with ramps
			
	To prevent back pain/injuries	To allow unit to load onto truck safely	To prevent drive-between-ramp accident

Step 4 – Load Deck

4.1 DRIVE UNIT ON TO TRUCK	A. Release unit See Basic 2	B. Lean out of window	C. Drive the unit to the correct position (using chock/drop-hole/visual judgement)
			
		To check alignment of unit on deck	To prevent drive-off and ensure enough clearance

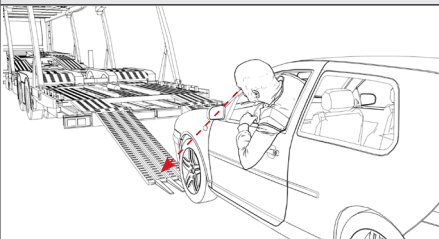
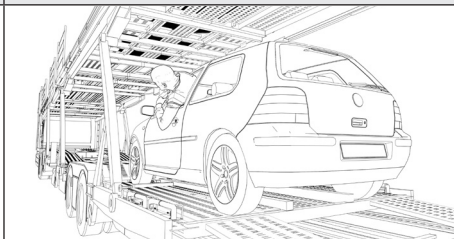
4.2* [IF UNIT PARKED INCORRECTLY]

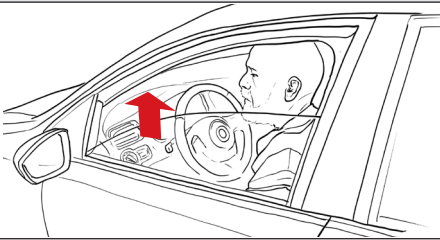
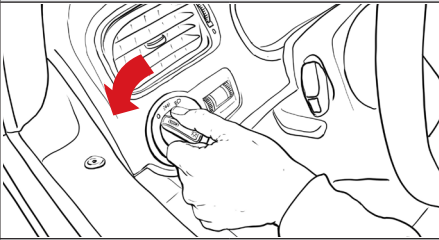
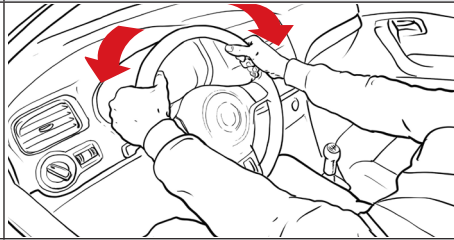
A. Drive unit off the truck		B. 4-step parking See Basic 3	
			
To re-configure chock position/drop-holes			
C. Exit Unit		D. Put on gloves	
			
To re-configure chock position/drop-holes		To prevent hand injuries	
E. If loading unit on truck upper deck See step 2A.3		OR	E. If loading unit on trailer upper deck See step 2B.2
		OR	E. If loading unit on truck/trailer lower deck See step 2C.2
F. Remove gloves		G. Enter Unit	
			
To enter unit without dirtying interior		To drive it onto truck	

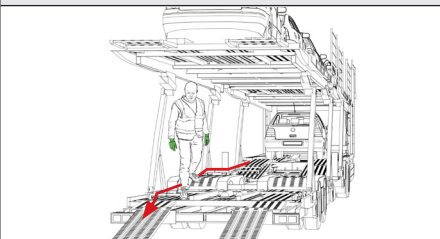
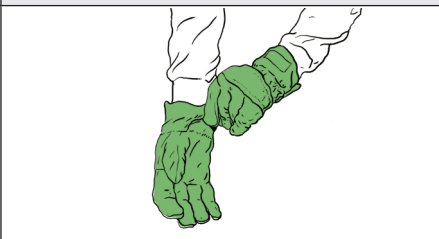
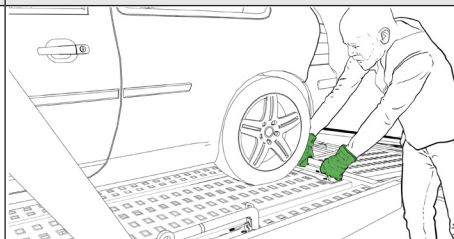
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Step 4 – Load Deck

Continued

4.2* [IF UNIT PARKED INCORRECTLY]	H. Release unit See Basic 2	I. Lean out of window	J. Drive the unit to the correct position (using chock/drop-hole/visual judgement)
		 To check alignment of unit on deck	 To prevent drive-off and ensure enough clearance

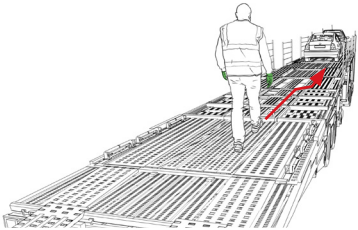
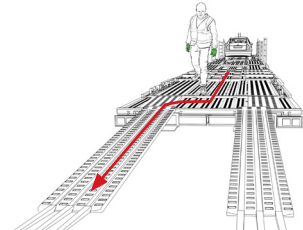
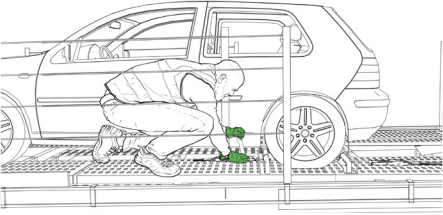
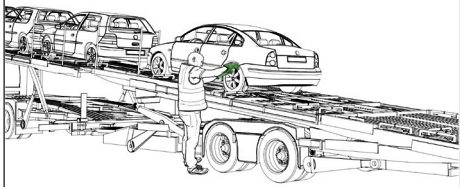
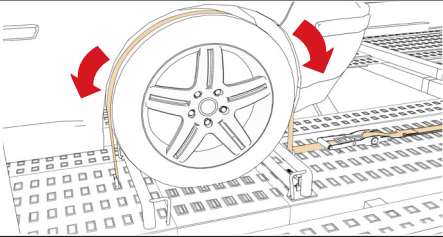
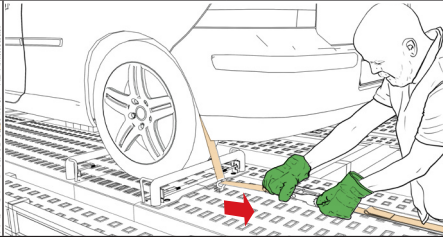
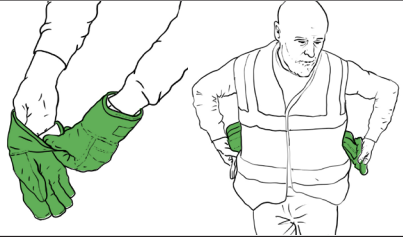
4.3 SECURE & EXIT UNIT	A. Close windows	B. Turn off lights	C. Straighten Steering Wheel
			
	To prevent damage to interior during transport	To preserve battery during loading & transport	To prevent damage to axle during transport
D. 4-step parking See Basic 3		E. Exit unit on deck See Basic 4	

4.4 APPLY CHOCK TO PREVENT ROLL-OFF	A. Walk to chocking point avoiding open holes, chocks or lashes	B. Put on gloves	C. Fix chock in direction of slope
	 To prevent slips, trips or falls	 To prevent hand injuries	 To prevent roll-off

Continued

Step 4 – Load Deck

Continued

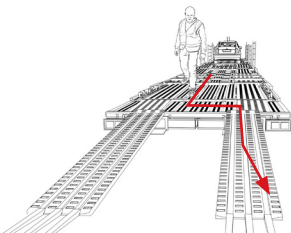

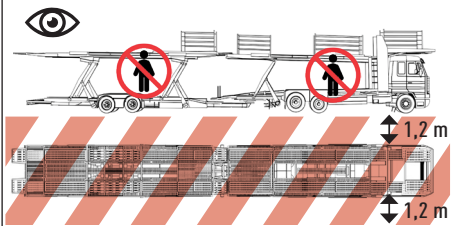
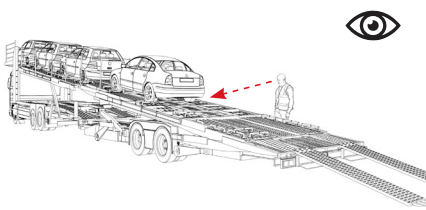
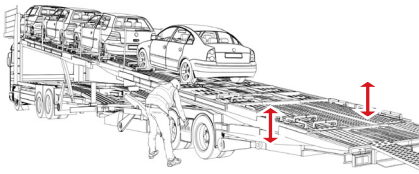

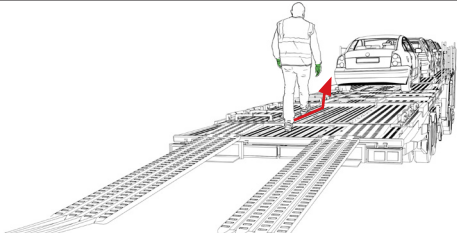
4.5* [IF UNIT AXLE NEEDS TO BE RELEASED] LASH FIXED AXLE	A. Walk to lashing point avoiding open holes, chocks or lashes		A. Walk off the deck avoiding open holes, chocks or lashes	
		Allowed only for units on Truck Upper Deck	OR Compulsory for units on Trailer Upper Deck & Truck/Trailer Lower Deck	
	To prevent slips, trips or falls		To prevent slips, trips or falls while walking to lashing point	
	B. Stand safely with body entirely between rails; if not possible to do so, lower the deck & lash from ground.		B. Stand safely on the ground; if unit not reachable, stand on lowest possible area of lower deck with 3 Points of Contact	
		Allowed only for units on Truck Upper Deck	OR Compulsory for units on Trailer Upper Deck & Truck/Trailer Lower Deck	
	To prevent fall-from-height		To prevent fall-from-height	
	C. Place lash over wheel without twisting	D. Tighten the lash		E. Remove gloves
				
	To ensure lash does not snap	To physically secure unit against roll-off		To enter unit without dirtying interior
	F. Release Unit See Basic 2		G. Exit Unit on Deck See Basic 4	

Continued

Step 4 – Load Deck

Continued

4.5* [IF UNIT AXLE NEEDS TO BE RELEASED] LASH FIXED AXLE

H. Walk to hydraulic control panel avoiding open holes, chocks or lashes	I. Put on gloves	J. Visually confirm that there are no people on or around the truck
		
To prevent slips, trips or falls	To prevent hand injuries	To prevent injuries to other people in the loading area
K. Visually ensure any units on deck are properly secured	L. Adjust decks or suspension using hydraulic levers	M. Remove gloves
		
To prevent roll-off accidents	To move unit/deck to desired location for clearance/ truck-driving	To re-enter unit without dirtying interior
N. Walk to unit avoiding open holes, chocks or lashes	O. Enter Unit See Basic 1	
		
To prevent slips, trips or falls		
P. 4-step parking See Basic 3		
Q. Exit Unit on Deck See Basic 4		

Continued

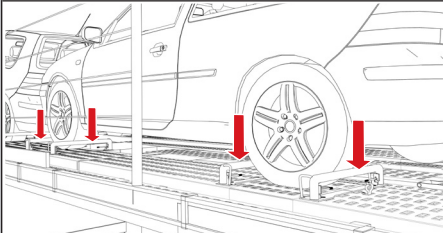
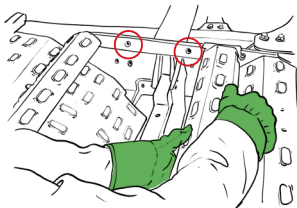
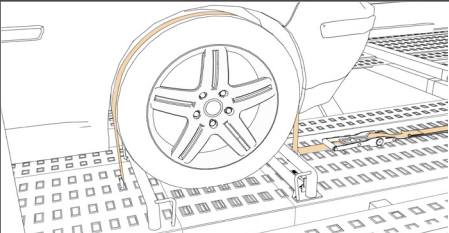
Step 4 – Load Deck

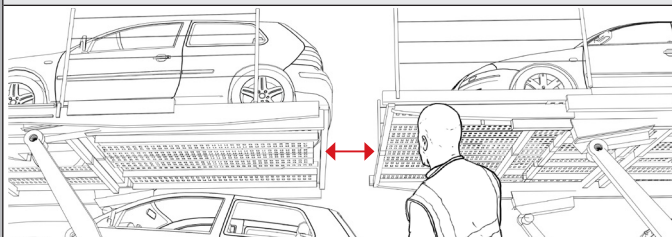
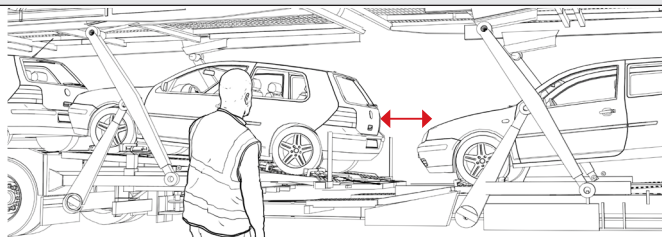
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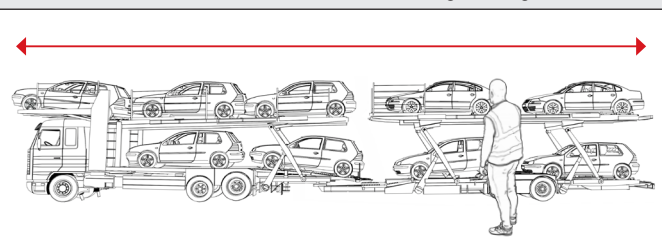
4.6 FINALIZE CHOCKS & LASHES ON UNIT	A. Walk to chocking point avoiding open holes, chocks or lashes		B. Put on gloves		C. Fix remaining chocks	
	To prevent slips, trips or falls		To prevent hand injuries		To ensure transport safety & quality	
	D. Stand in a safe position (body entirely between rails); if not possible lower deck & lash from ground.			D. Stand safely on the ground; if not possible, stand on lowest possible area of lower deck with 3PC to lash		
			Allowed only for units on Truck Upper Deck		Compulsory for units on Trailer Upper Deck & Truck/Trailer Lower Deck	
	To prevent fall-from-height		OR		To prevent fall-from-height	
	E. Place lash over wheel without twisting		F. Tighten the lash		G. Remove gloves	
	To ensure lash does not snap		To physically secure unit against roll-off		To prevent dirtying unit interior	
4.7 PREPARE FOR NEXT UNIT	Prepare Truck Upper Deck See Step 2A		OR		Prepare Trailer Upper Deck See Step 2B	
					OR	
					Prepare Trailer Upper Deck See Step 2C	
4.8 LASHING FIRST UNIT	A. For cars loaded in first position on truck-upper decks, do NOT lash the axle closest to the truck's front edge					
	To avoid fall-from-height					

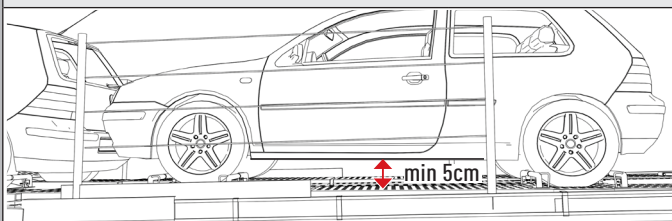
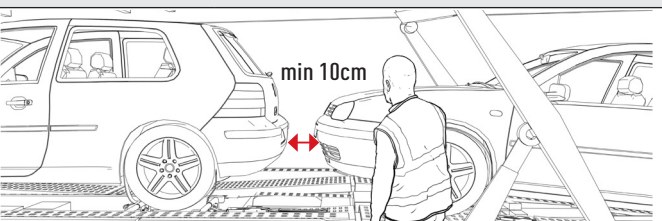
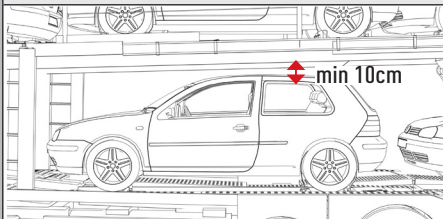
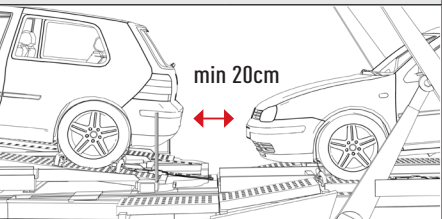
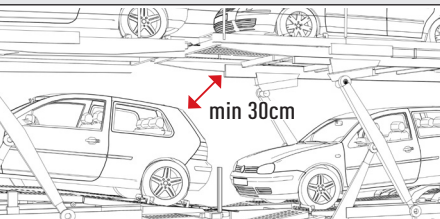
*In certain countries (e.g. Germany) there is a legal requirement to lash diagonal axles of car in the first position. For trucks loading/unloading in such countries, recommendation 1C is not applicable. In such cases, drivers can lash the foremost axle as long as they stand entirely between safety rails, facing the front of the truck.

5. Final Checks

5.1 CHECK CHOCKS & LASHES	A. Visually check all units have required number of chocks	B. Check that any unit in drop-holes have at least 1/6th wheel in the hole	C. Check that all units have required number of lashes
			
	To ensure units do not move laterally during transport	To ensure units are secure during transport	To ensure units do not bounce during transport


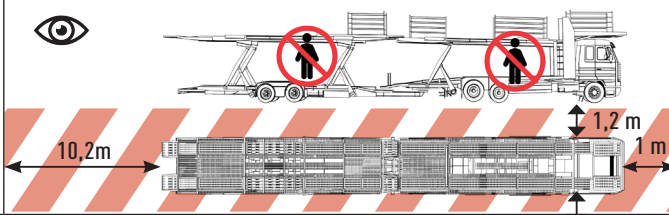
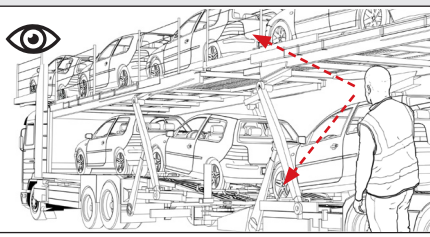
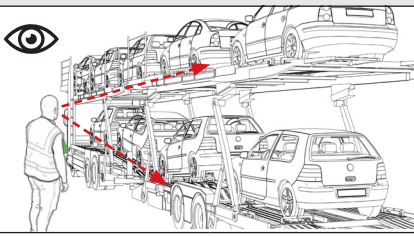
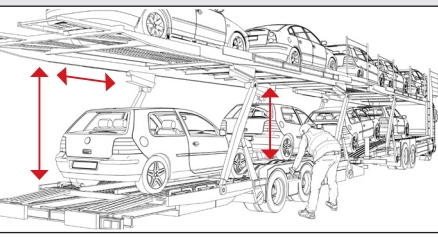
5.2 CHECK POSITION OF DECK	A. Ensure gap between truck & trailer upper decks is sufficient	B. Ensure gap between truck & trailer lower decks is sufficient
		
	To allow truck to turn	To allow truck to turn

5.3 CHECK HEIGHT & LENGTH	C. Check height of load using a height-measure stick	D. Check length of load including overhang if any allowed (follow the method learned during training)
		
	To prevent roof damages to units on upper deck	To prevent damage to units while turning

5.4 CHECK CLEARANCES	A. Check underbody clearance for each unit is at least 5 cm		B. Check bumper-to-bumper clearance for each unit is at least 10 cm	
				
	To prevent underbody damage		To prevent bumper damage	
	C. Check roof clearance for units on lower deck is at least 10 cm	D. Check clearance between last unit on trailer & first unit on truck is at least 20 cm	E. Check clearance between diagonal units on lower & upper decks is at least 30 cm	
				
	To prevent roof damage	To prevent damage when truck turns	To prevent roof to bumper damage when going over wheel humps	

5. Final Checks

Continued

5.5 [IF DECK POSITION, HEIGHT/LENGTH, CLEARANCES NOT OK] ADJUST DECKS	A. Put on gloves (gloves should be in trouser/jacket pockets when not in use)		B. Visually confirm that there are no people on or around the truck	
				
	To prevent underbody damage		To prevent injuries to other people in the loading area	
	C. Re-confirm that all units on all decks are secured as required	D. Visually check that there are no loose objects on any deck	E. Adjust the decks	
				
	To prevent roll-off	To prevent injuries during manipulation	To achieve required position, height/length, clearances	
	F. Confirm gap between truck trailer decks	G. Re-confirm trailer height/length	H. Re-confirm clearances between units	
	Check position of deck See step 5.2	Check height and length See step 5.3	Check height and length See step 5.4	
	To allow truck to turn without damaging units	To prevent damage to units on upper deck/last units on decks	To prevent damage to units	

Process Check Sheet

Auditor:

Site:

Check Date:

Step		Key Point	Unit									
			1	2	3	4	5	6	7	8	9	10
Fall-from-height Prevention		A. Visually confirm safety rails are present & damage-free										
		B. Lower decks as much as possible when loading top decks										
		C. Never lash axle closest to front-end of truck	No check required.									
		D. Lashing from safe position: Truck upper deck: Body entirely between safety rails with 3 PC OR from ground; Trailer upper deck/Entire lower deck: Lash from ground										

Slips & Trips Prevention		A. Use 3 points of contact when moving on deck										
		B. Check + prepare decks to avoid walking near holes, chocks or lashes										
		C. Walk on designated safe areas on the truck as specified by truck manufacturer										
		D. NEVER run, jump or walk backwards.										

Roll-off Prevention		A. Follow 4-step parking & CONFIRM car securely parked										
		B. Chock car in direction of slope, as soon as possible after exiting										
		C. Lash fixed axle BEFORE releasing car brake/gear for deck manipulation										

Drive-Off Prevention		A. Place a chock, or open a drop-hole to physically prevent drive-off	No check required.									
		B. Visually confirm alignment to truck centre-line & stopping position when driving unit on to deck										

Entrapment Prevention		A. Ensure no one is on/around truck before manipulating decks, ramps or suspension										
		B. Ensure all safety pins & levers are locked										

Disclaimer

These guidelines summarize the principles of safe loading on car transporters. While broadly encompassing, they are not applicable to all scenarios/situations that may arise during loading activities. Use common sense.

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